



# Train Whistle Cessation – Update

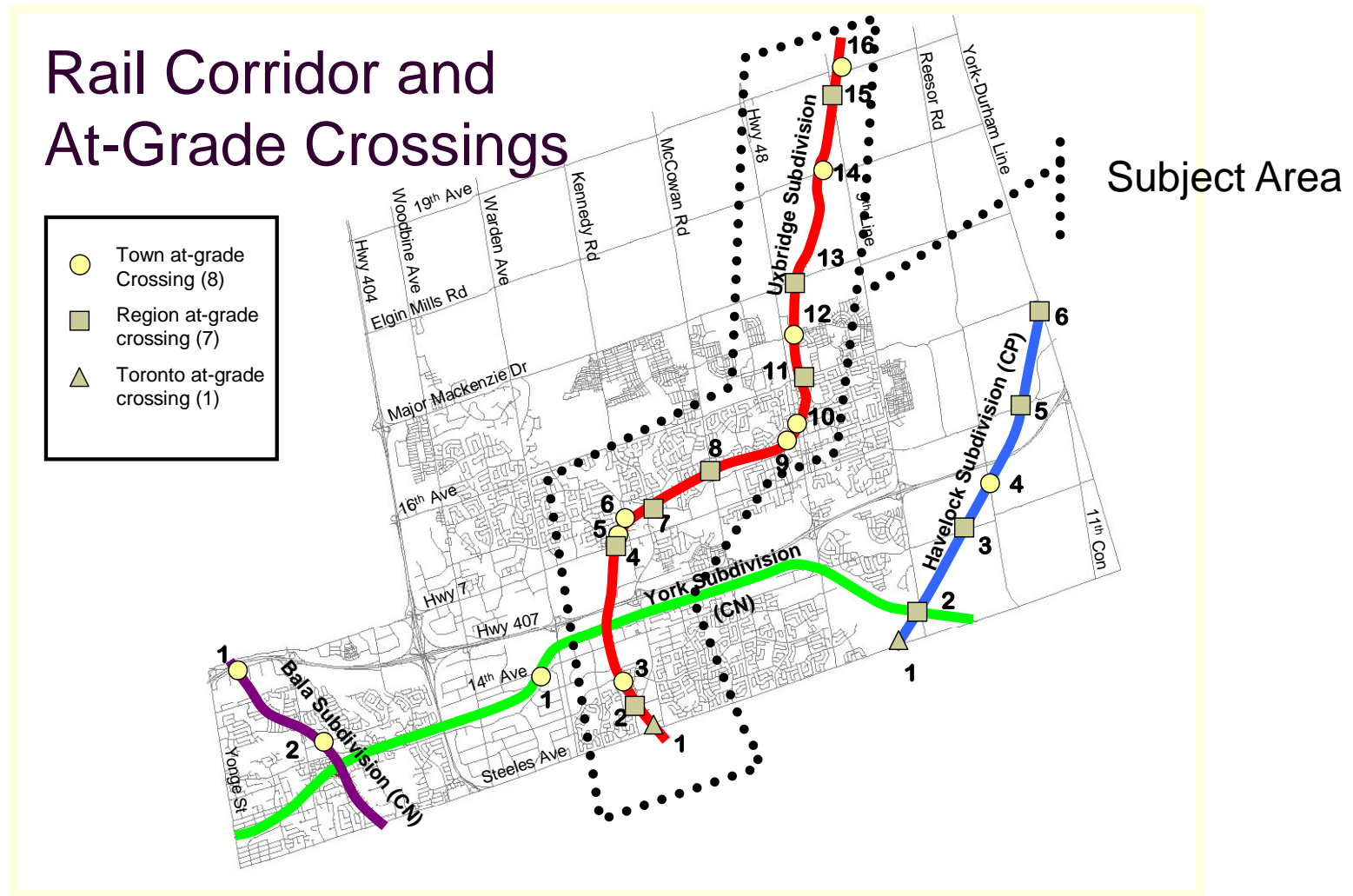
Development Services Committee  
March 3, 2009

# Background

- On-going resident concerns
  - Noise issue relating to train whistling at level crossings
- Council at June 10, 2008 meeting:
  - Endorsed the implementation of train anti-whistle by-laws with priority given to 13 level crossings located within Markham's urban boundary
  - Requested that the Region of York and Toronto implement anti-whistling by-laws at their crossings within the Town of Markham
  - Requested that the Region of York amend their policy to accept train whistle cessation periods other than overnight and that the Region of York be responsible for costs, risks and liability at their level crossings

# Background

## Uxbridge Train Line



# Background

## Uxbridge Train Line Facts

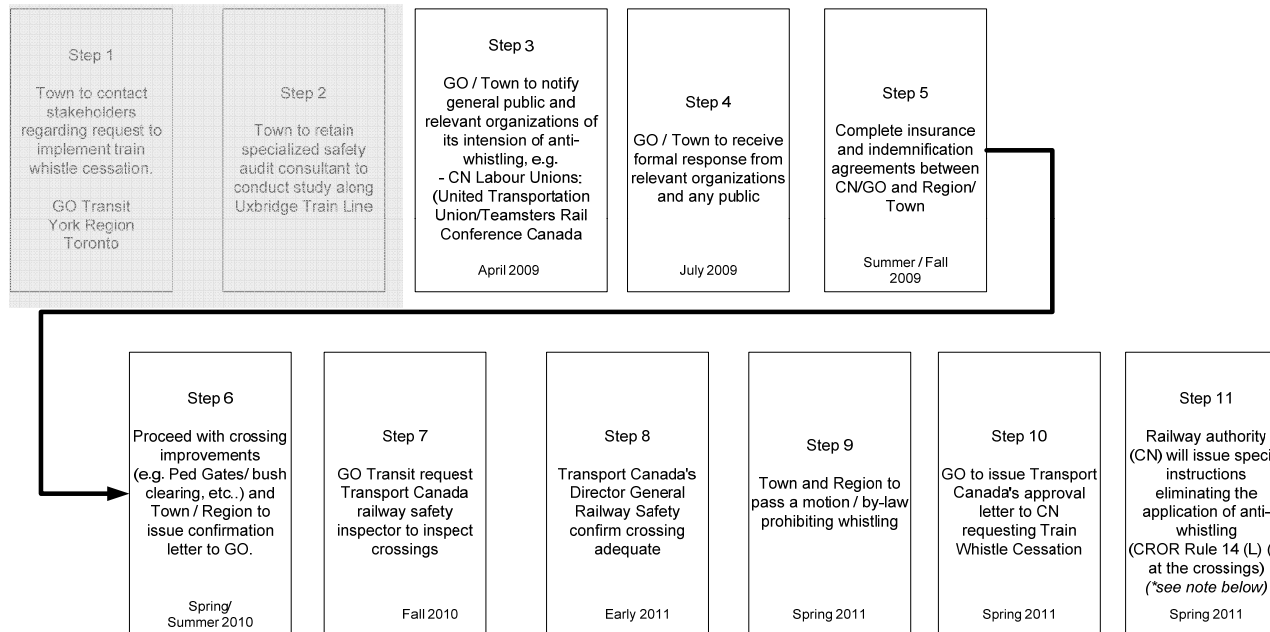
- 16 level train crossing of which 12 travel through existing communities
- Of the 16 level train crossing
  - 7 York Region
  - 1 Toronto (Steeles Av with existing train whistle cessation)
  - 8 Town
- Currently 10 commuter trains
  - Five during AM period
  - Five during PM period
- Metrolinx recommends all day service in their 15 year program
  - Number of trains expected to increase from 10 to approximately 17 trains per day

# York Region's

## Anti-whistle Policy for Trains at Level Crossings

- Regional Council at its meeting on June 19, 2008 adopted a new Anti-whistle Warrant Criteria policy that:
  - entertains anti-whistling by-law requests from the local municipalities with different hours of restrictions other than the original overnight restriction between 10:00 pm and 6:00 am;
  - requires a safety audit by a specialized consultant and the audit be approved by Transport Canada;
  - requires pedestrian gates (where pedestrian volumes exist) and associated cost will be the responsibility of the Region at Regional train crossings;
  - an educational program is developed for the affected area;
  - the Region will be responsible for risk and liability at train crossings on Regional roads.

# Implementation Process to Train Whistle Cessation



**\*Note:** In accordance with Canadian Rail Operating Rules, the train operator still has discretionary use of the train whistle in emergency situations.

**Reference:** Transport Canada – Procedure and Conditions for Eliminating Whistling at Public Crossings  
[www.tc.gc.ca/railway/guideline/eliminatingwhistling.htm](http://www.tc.gc.ca/railway/guideline/eliminatingwhistling.htm)

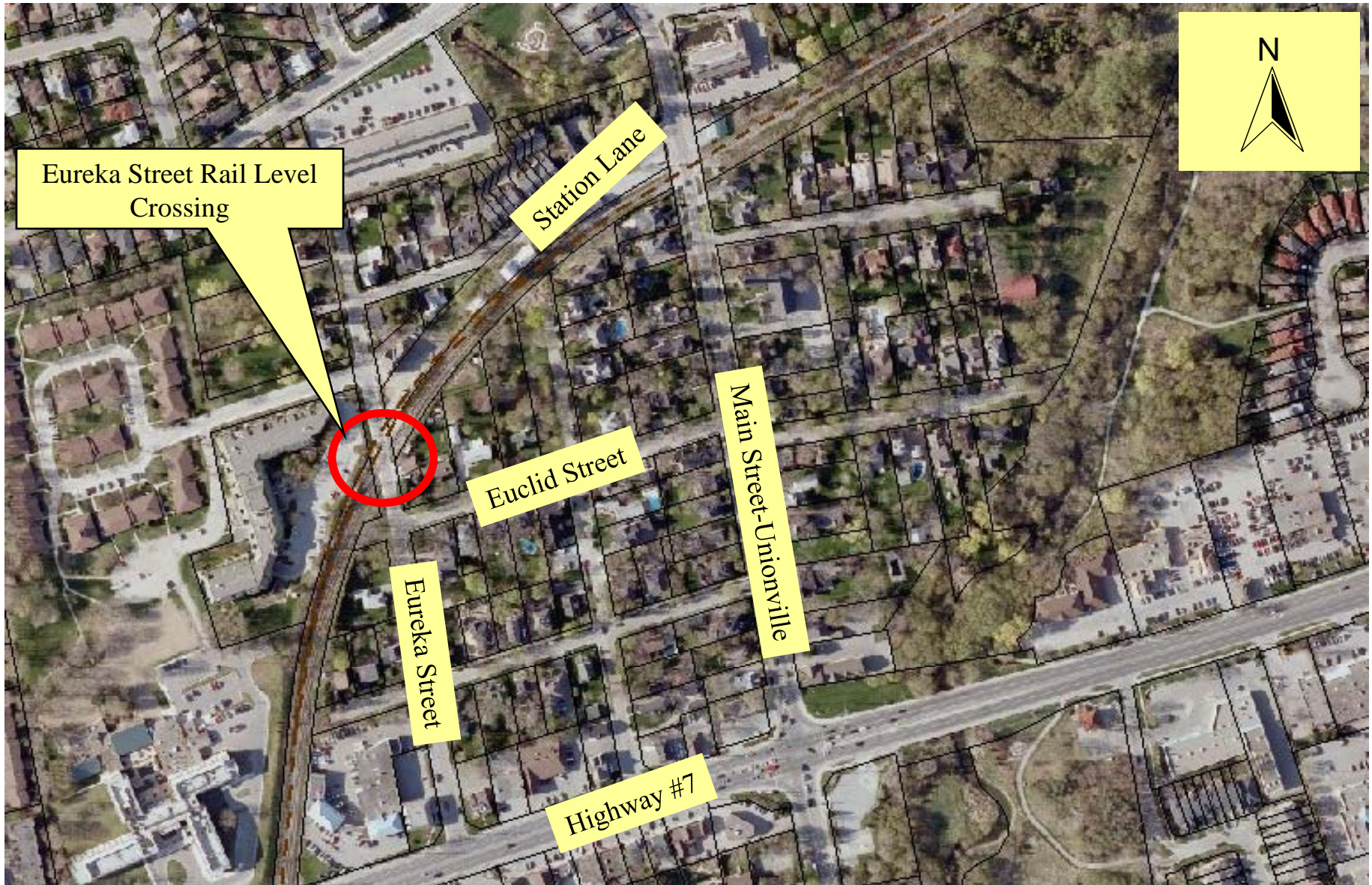


## Consultant's Safety Audit to Implement Train Whistle Cessation

- In order to implement train whistle cessation, staff retained a consultant in August 2008 to identify safety deficiencies and mitigation measures at level crossings along the Uxbridge rail corridor
- Consultant completed the safety audit in October 2008 and have recommended the following:
  - General mitigation measures:
    - signs and pavement installations / relocations
    - sightlines to be corrected
  - Specific mitigation measures:
    - Eureka Street – **see next slide**
    - Bur Oak Avenue – re-grade existing approaches ( 2% grade) - to be finalized
- **Consultant identified that train whistle cessation can be implemented provided that the deficiencies be corrected**



## Eureka Street Rail Level Crossing







## Specific Mitigation Measures for Eureka Street

- Residential driveway proximity to train crossing causing sightline concerns
- *Mitigation Measure Options:*
  - *Option 1 – relocate residential driveway and garage*
  - *Option 2 – close road by providing barriers on either side of crossing*





# Estimated Costs to Implement Train Whistle Cessation

| Mitigation Measures   | Remove and Trim Brush             | Install / Repair Farm Gates   | Install / Repair / Modify Fencing | Warning Signs and Pavement Markings | Pedestrian Gates                        | Driveway Relocation                            | Road Approach Re-grading / Rehabilitation | Consultant's Safety Audit | Education Program                                  | Total                         |
|-----------------------|-----------------------------------|-------------------------------|-----------------------------------|-------------------------------------|---|--|---|---------------------------|--|-------------------------------|
| <b>1. GO Transit</b>  | \$5,500 (11 cross. At \$500 Each) | \$8,000 (4 cross. at \$2,000) | \$5,000 (5 Cross. s at \$1000)    | \$5000 (10 at \$500 each)           | n/a                                     | n/a  | \$6,000 (3 Farm Crossings at \$2000 each) | n/a                       | n/a  | <b>\$29,500</b>               |
| <b>2. York Region</b> | n/a                               | n/a                           | n/a                               | n/a                                 | \$500,000 (5 Locations at \$100 k each) | n/a  | n/a                                       | n/a                       | n/a  | <b>\$500,000</b>              |
| <b>3. Town</b>        | n/a                               | n/a                           | n/a                               | \$75,000 (150 each at \$500 each)   | \$400,000 (4 Locations at \$100 k each) | <b>\$50,000-100,000 (Options and Cost TBF)</b> | \$50,000 Bur Oak Avenue                   | \$30,000                  | \$25,000 (covers both Town and Regional Crossings) | <b>\$630,000 to \$680,000</b> |
| <b>Total</b>          | \$5,500                           | \$8,000                       | \$5,000                           | \$80,000                            | \$900,000                               | \$30,000                                       | \$56,000                                  | \$30,000                  | \$25,000   | <b>\$1,209,500</b>            |

## Budget Notes:

### 1. Town

- Town Current Budget: **\$182,100**
- Requested Budget in 2009: **\$317,900** (combined current and 2009 budgets = \$500,000)
- Additional funding required 2010: **\$130,000 to \$180,000**

### 2. Region – no designated funding budgeted to-date

### 3. GO Transit expected to complete mitigation measures by Spring 2009

# Next Steps

- Staff proceed with the steps identified in this report
- Finalize Regional funding (\$500,000), commitment, Whistle Cessation times and scheduling
- Update Council in Fall 2009 as to project status:
  - Region of York funding approval and whistle cessation times
  - Responses from the general public and comments from agencies (CN/GO and York Region)
  - Insurance and indemnification agreements
  - Update schedule
  - Eureka Street options
  - Bur Oak re-grading